

Prague Integrated Transport



*Increasing the Quality
of Public Transport in Prague*

Basic Data

Capital of Prague

City area 496 sq. km

Population 1,166,000

Job opportunities about 780,000

Length of road network 3,520 km

Basic Data

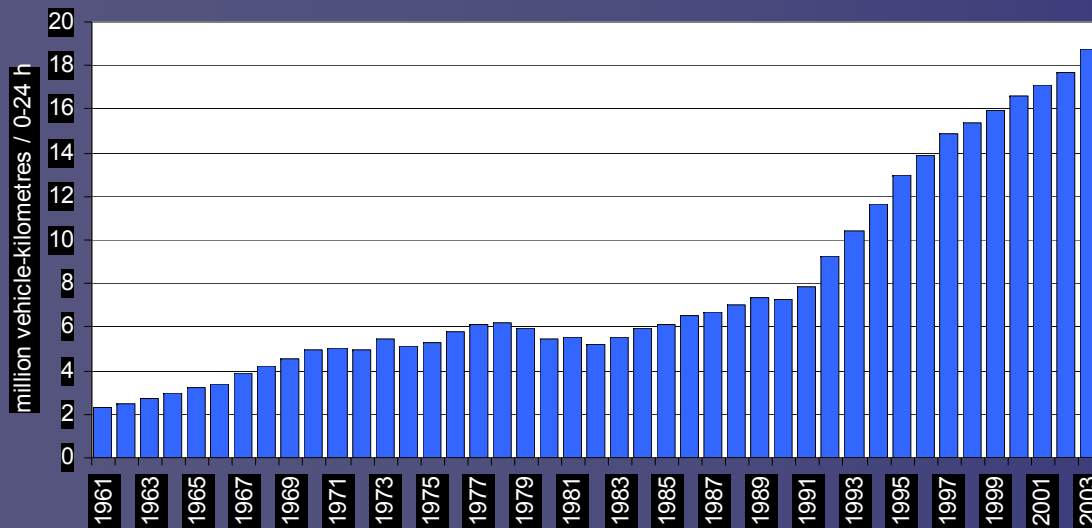
Capital of Prague

| | |
|---|----------|
| Metro (underground) network (in operation) | 49.8 km |
| Tram network | 140.9 km |
| specifically, tramlines on its own track bed | 52 % |
| Municipal Public Transport bus network within the city | 686.1 km |

Impact of post-1989 changes on the transport system

Motor-car traffic vehicle-kilometres development 1961 - 2003

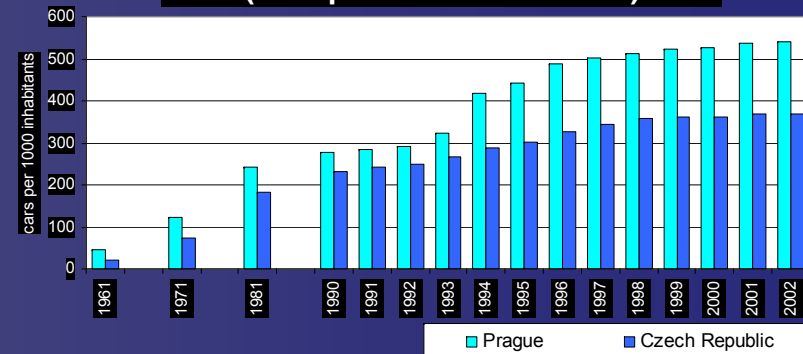
The whole of the road network, an average workday, 0 - 24 h



Modal split:

57% public transport
43% individual car

Personal car motorization 1961 - 2003 (cars per 1000 inhabitants)



Changes in Financing Policy

until 1989

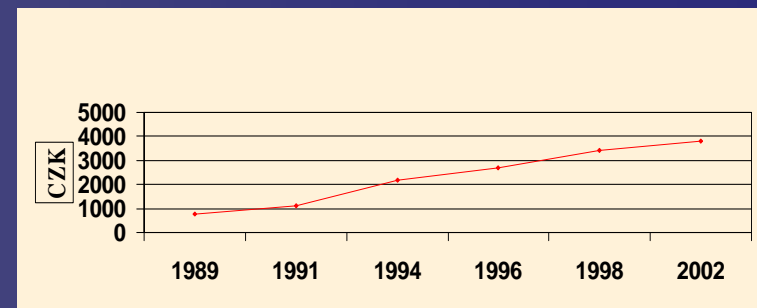
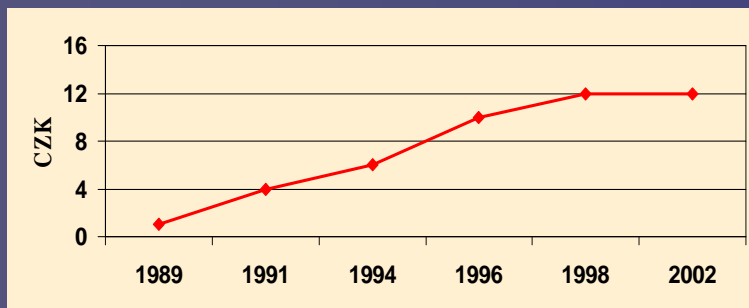
- the central government funded all capital investments and operating subsidies

after 1989

- most of this funding transferred to local municipal authorities

from 1995

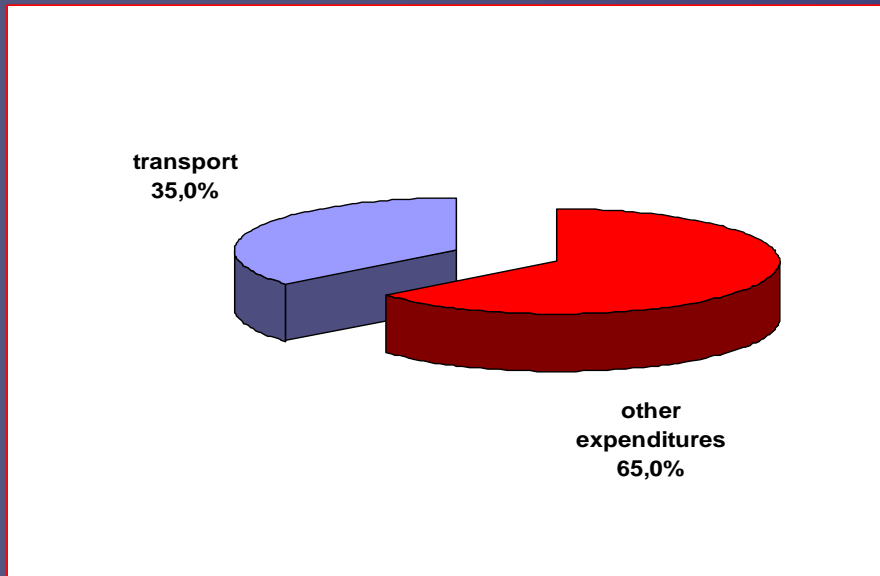
- state financial aid programme to support rolling stock renewal and urban transport construction



Prague Municipal Budget in 2003

The budget chapters sharing in the
adjusted 2003 budget expenditures

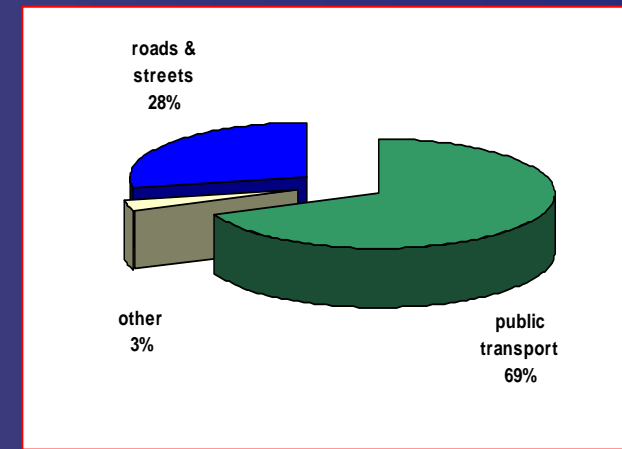
1.46 BILLION €



Transport chapter: 0.51 billion €



Total transport expenditures



Transport Policy Principles of the City of Prague



principles declared are closely related to operation and development of Prague PT system

- to design, develop and manage transport structure as a complex system of all transport modes that have to cooperate rationally
- development of transport system not only in terms of transport capacity but also in terms of safety and provision of sufficient quality
- to prefer operation and development of environmentally friendly transport system
- to ensure safe pedestrian transport and provide a transport system acceptable for persons with impaired mobility and low orientation ability
- to ensure transport system development in harmony with residential area development to aim at decreasing of transport demand in the city through urban planning

Prague Integrated Transport

- Organising Authority:

ROPID - Regional Organizer of Prague Integrated Transport
- established in 1993
- 50 employees

- Prague Integrated Transport Area Prague

(496km², population 1.2 mil.)

+ greater part of the Central Bohemian Region
(the boundary to 50 km from the centre, population approx. 0.5 mil.)

Metro, Tram and Railways of Prague Integrated Transport

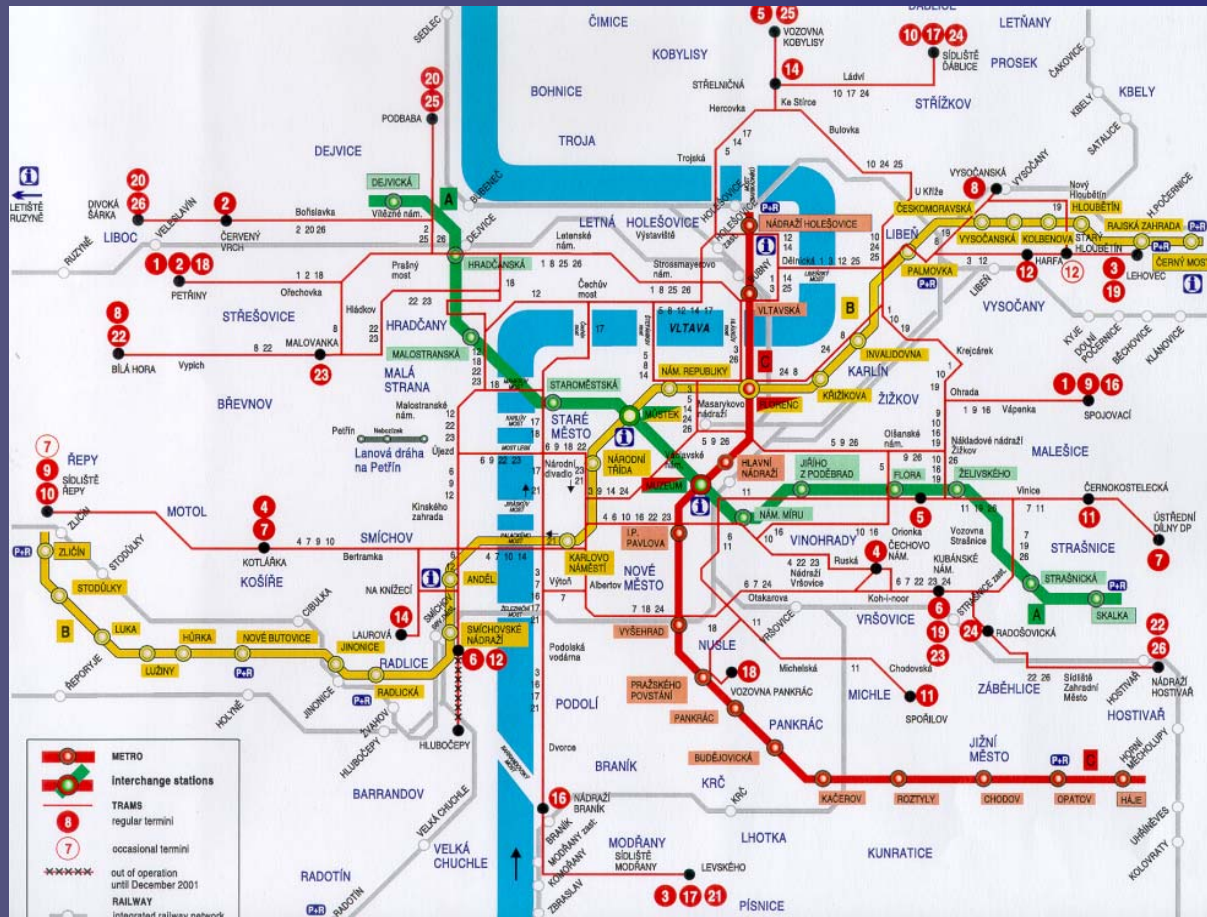
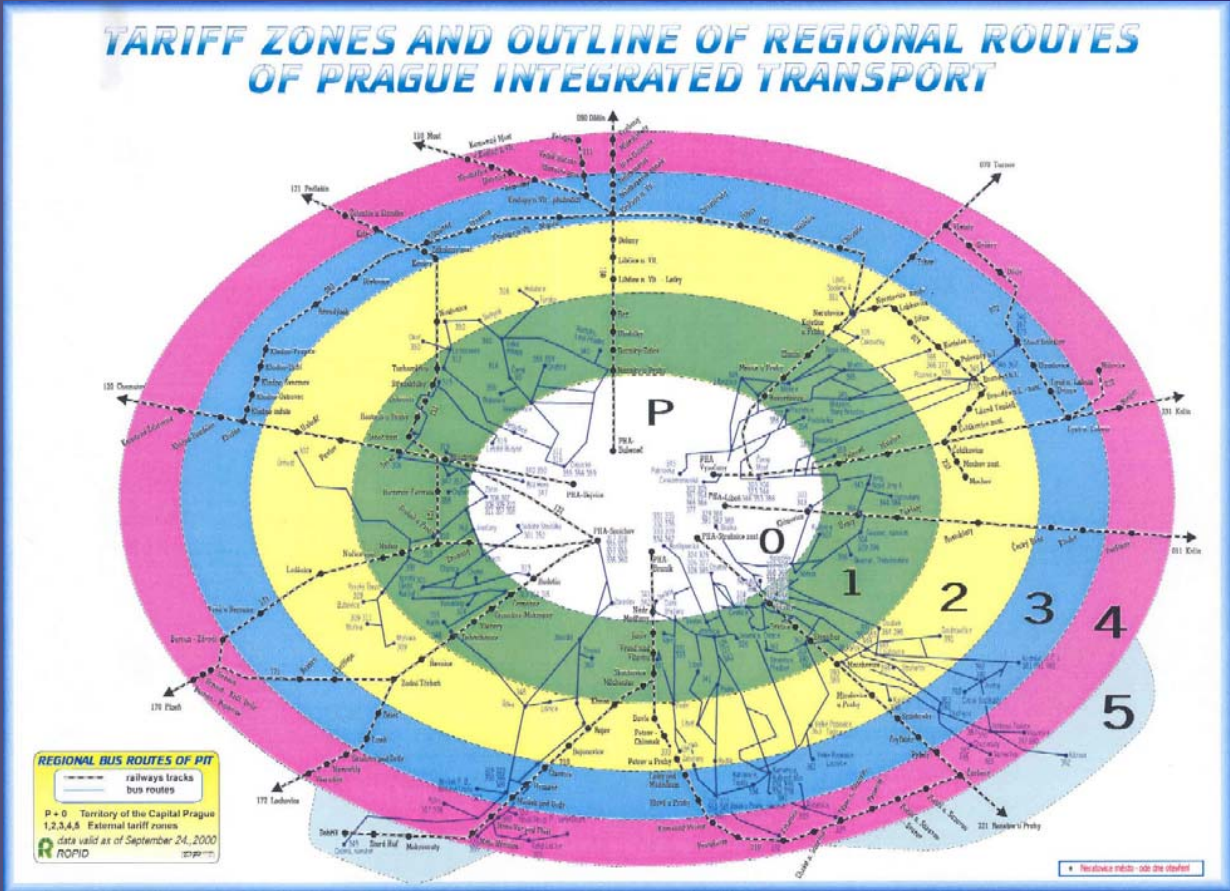




Chart of Fare Zones of Prague Integrated Transport










Development of PIT

Basic data

| | 1995 | 1996 | 1997 | 1998 | 1999 | 2000 | 2001 | 2002 | 2003 |
|---|------|------|------|------|------|------|------|------|------|
| <i>Number of municipalities serviced by PIT lines</i> | 15 | 55 | 69 | 83 | 104 | 159 | 218 | 259 | 278 |
| <i>Number of regional bus routes included in PIT</i> | 11 | 31 | 38 | 48 | 54 | 89 | 114 | 131 | 146 |
| <i>Number of railway stations integrated into PIT</i> | 59 | 181 | 181 | 181 | 181 | 190 | 200 | 219 | 221 |
| <i>Share of PIT tickets used on railway included in PIT (%)</i> | 25.5 | 29.1 | 32.5 | 35.6 | 37.2 | 39.2 | 43 | 52.1 | 56.5 |



Contracts on supply of transport services in PIT negotiated with

| Operator | Operated routes |
|--|---|
| Dopravní podnik hl. m. Prahy, akciová společnost  | metro: 3 lines funicular: 1 line tram: 24 city lines + 8 night lines bus: 148 city lines + 12 night lines 20 school lines + 23 suburban lines |
| Milan Bado | bus: 1 city line |
| Jaroslav Štěpánek | bus: 1 city l. + 1 school l. + 1 suburban l. |
| Jana Bamatová - SPOJBUS  | bus: 5 suburban lines |
| Martin Uher  | bus: 1 city line + 11 suburban lines |
| Hotliner s. r. o. | bus: 8 suburban lines |
| ČSAD Střední Čechy, spol. s r. o.  | bus: 23 suburban lines |
| ČSAD Praha - západ, s. p. | bus: 2 city lines + 3 suburban lines |
| ČSAD Praha Vršovice a. s.  | bus: 1 city line + 26 suburban lines |
| ČSAD Kladno a. s.  | bus: 5 suburban lines |
| ČSAD POLKOST, spol. s r. o.  | bus: 9 suburban lines |
| ČSAD Benešov a. s.  | bus: 1 suburban line |
| Okresní autobusová doprava Kolín s. r. o.  | bus: 2 suburban lines |
| PROBO TRANS BEROUN, spol. s r. o.  | bus: 2 suburban lines |

České dráhy



- 400 trains on working day
- 25 lines within PIT

Operators of newly introduced bus routes are selected through public tenders.



Service Quality Programme of Prague Public Transit Co. Inc.

Implementation of this programme in co-operation
with RATP  from the beginning of 1998

Based on European Standard

**"Transportation - Logistic and Services - Public Passenger
Transport - Service Quality Definition, Targeting and
Measurement"**

(EN 13 816:2002 – Czech Republic member of CEN)

QUALITY CRITERIA

The criteria represent the customer view of the service provided and have been divided into 8 categories:

Level 1

- 1) availability
- 2) accessibility
- 3) information
- 4) time
- 5) customer care
- 6) comfort
- 7) security
- 8) environmental impact

Level 2

- 3.2. Travel information
normal conditions

Level 3

- 3.2.1 street direction
- 3.2.2 b/a - point identification
- 3.2.3 vehicle direction signs
- 3.2.4 about route
- 3.2.5 about time
- 3.2.6 about fare
- 3.2.7 about type of ticket

Service Quality Standard of Prague Public Transit Co. Inc.

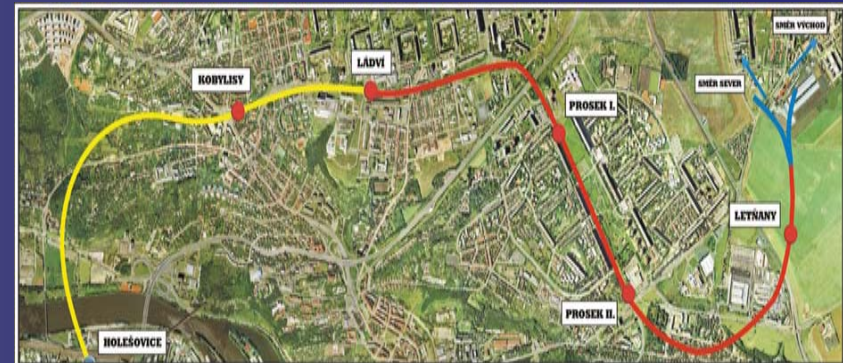
- Punctuality/regularity
- Passenger information
- Customer care
- Uniform discipline
- Operability of ticket vending machines (*since 2000*)
- Operability of metro stations equipments for people with reduced mobility (*since 2004*)



- Standards defined through:
 - Service reference sample
 - Desired level of achievement
 - Unacceptable situations

Enhancing services

subway, bus & tram



LEGENDA:

- TRASA METRA IV C1
- TRASA METRA IV C2 - TRASA NAVRŽENÁ PRO ZMĚNU ÚZEMNÍHO PLÁNU
- NAVRŽENÉ SMĚRY ROZVOJE



Services Designed for People with impaired mobility and low orientation ability

The blind inquires for number of route and direction of tram link



Acoustic orientation beacon with command receiver PPN 01M installed near the entrance of underground



Public Transport and City Development in Prague

